

SCOPE OF WORK

THIS PROJECT WILL MAKE REPAIRS THE RAIL ROAD CROSSING AT THREE PUBLIC ROADS: NC HIGHWAY 24, RIGGS ROAD AND MORRISTOWN ROAD.

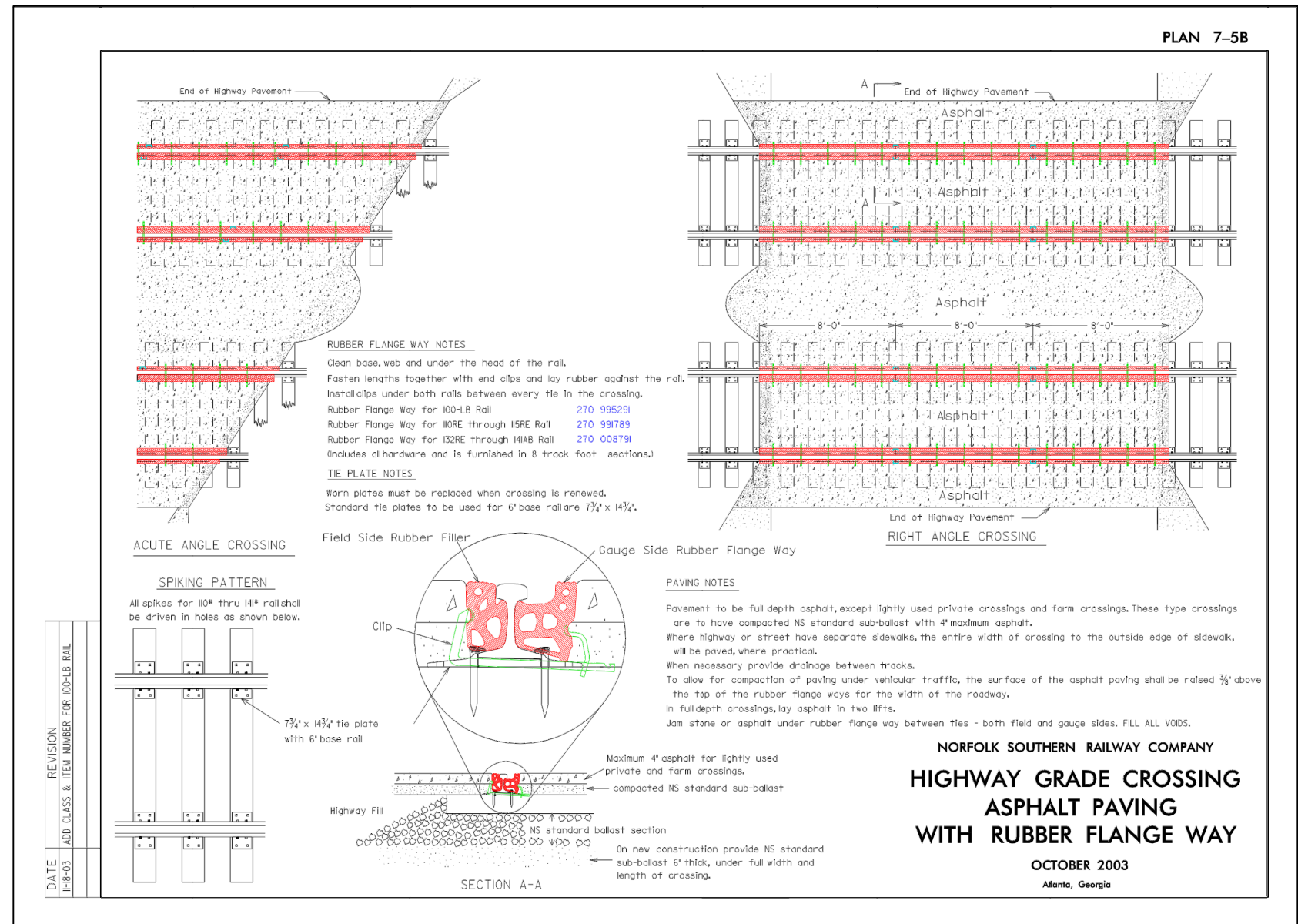
1. REMOVE EXISTING RAIL FROM NEAREST JOINT OF TRACK SECTION TO THE ROADWAY. PROTECT AND REINSTALL. RAIL REMOVED WILL BE REUSED.
2. REMOVE COMPONENTS OF EXISTING RAIL ROAD CROSSING INCLUDING TIES AND CONCRETE PANELS. SAW CUT ASPHALT PAVEMENT A MINIMUM OF 10 FEET FROM THE CENTERLINE OF THE RAIL ROAD TRACK.
3. ENSURE A NORFOLK SOUTHERN 9 INCH SUB-BALLAST UNDER FULL WIDTH AND LENGTH OF CROSSING.
4. INSTALL NEW 9 INCH THICK NORFOLK SOUTHERN BALLAST SECTION. INSTALL NEW 7" x 9" x 8" 6" WOODEN TIES. INSTALL NEW TIE PLATES AND SPIKES. REUSE AND REINSTALL EXISTING RAIL.
5. INSTALL NEW RUBBER FLANGE WAY PER NORFOLK SOUTHERN HIGHWAY GRADE CROSSING DETAIL PLAN 7-5B.
6. REPAVE ROADWAY WITH 1.5 INCH SURFACE COURSE OF S9.5C ASPHALT OVER A 2.5 INCH INTERMEDIATE COURSE OF I19.0C ASPHALT OVER A 3 INCH BASE COURSE OF B25.0C ASPHALT OVER 8 INCH COMPACTED AGGREGATE BASE COURSE. THE SURFACE OF THE ASPHALT PAVING SHALL BE RAISED $\frac{3}{8}$ " ABOVE THE TOP OF THE RUBBER FLANGE FOR THE WIDTH OF THE HIGHWAY. CROWN ROADWAY FOR POSITIVE DRAINAGE.
7. MATCH EXISTING HIGHWAY PAVEMENT ELEVATIONS AND GRADE. MATCH EXISTING RAILROAD ELEVATION AND GRADE.
8. REMARK NEW PAVEMENT. MATCH EXISTING PAVEMENT MARKS.

GENERAL NOTES

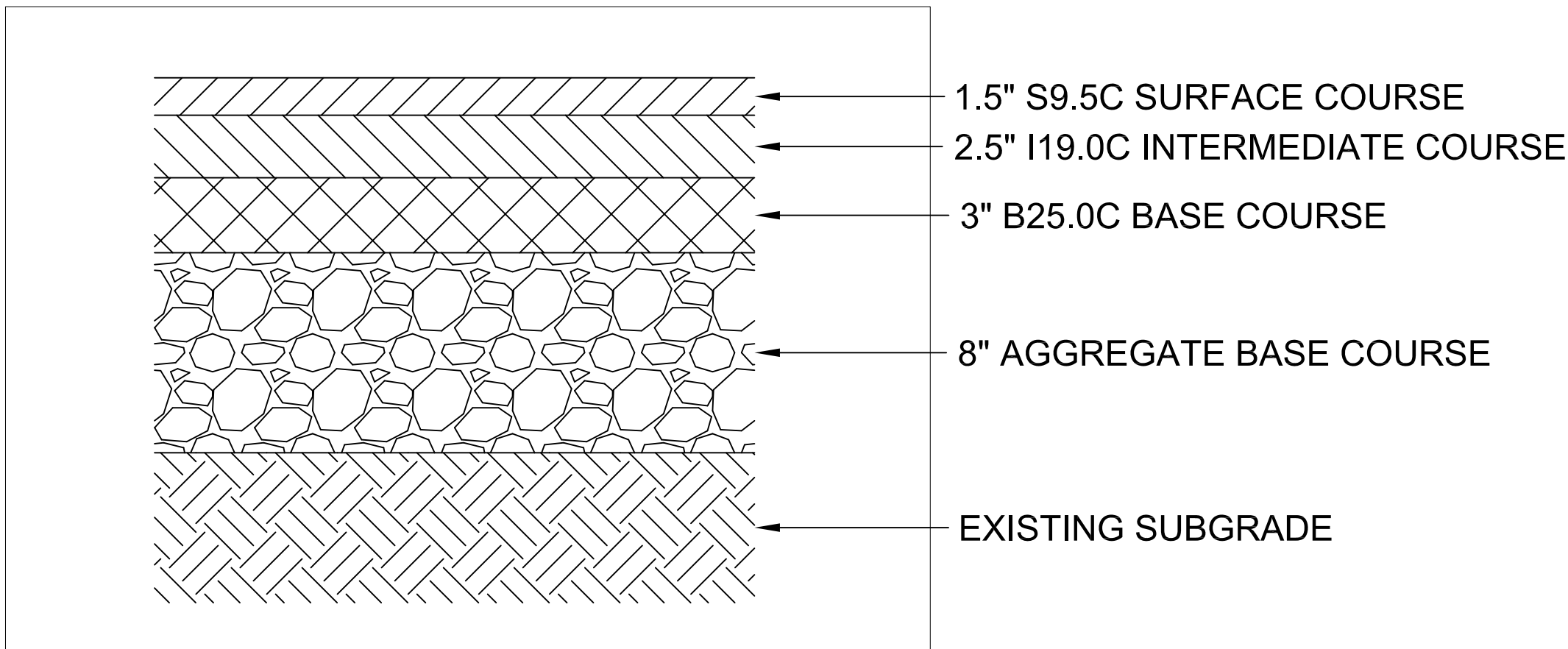
1. CONTRACTOR IS RESPONSIBLE FOR ALL ROAD CLOSURES. ROAD CLOSURES WILL BE IN ACCORDANCE WITH THE MUCTD. CONTRACTOR IS PERMITTED TO SHUT DOWN EITHER THE EAST BOUND OR WEST BOUND OF NC HIGHWAY 24 BUT NOT BOTH AT THE SAME TIME. CONTRACTOR SHALL DIVERT TRAFFIC ONTO THE OPPOSITE SIDE OF THE NC HIGHWAY 24 IN ACCORDANCE WITH MUCTD. CONTRACTOR MAY HAVE THE ENTIRE ROADWAY AT RIGGS ROAD AND MORRISTOWN ROAD. CONTRACTOR IS RESPONSIBLE FOR ALL SIGNAGE AND TEMPORARY TRAFFIC CONTROL MEASURES.
2. CONTRACTOR SHALL COMPLETE ALL WORK AT NC HIGHWAY 24 (BOTH LANES) IN A CONTINUOUS FOUR WEEK PERIOD. CONTRACTOR IS AUTHORIZED TO WORK MULTIPLE SHIFTS, OVERTIME, NIGHTS AND WEEKENDS TO ACCOMPLISH WORK AT NC HIGHWAY 24 WITHIN THOSE FOUR WEEKS.
3. NIGHT WORK AT RIGGS ROAD AND MORRISTOWN ROAD IS NOT AUTHORIZED.
4. CONTRACTOR MAY REUSE SUB-BALLAST AND BALLAST PROVIDED THEY MEET CURRENT SPECIFICATIONS.
5. CONTRACTOR SHALL LOCATE ALL UNDERGROUND UTILITIES PRIOR TO GROUND DISTURBING ACTIVITIES.
6. CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS. THE MEASUREMENT AND QUANTITIES FURNISHED HERE ARE FOR REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR ACTUAL FIELD VERIFICATION PRIOR TO BIDDING, ORDERING MATERIALS AND DURING EVERY STEP OF CONSTRUCTION FOR EXISTING SURFACES, DIMENSIONS AND CONDITIONS.
7. PRESERVE AND PROTECT ALL EXISTING UTILITIES. CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES. REPAIR OR REPLACEMENT OF DAMAGED UTILITIES SHALL BE INSPECTED BY THE OWNER OF THE REPAIRED UTILITY BEFORE COVERING UP WORK WITH DIRT OF OTHER MATERIAL.
8. CONTRACTOR WILL ACCESS WORK SITES USING EXISTING RIGHT OF WAYS OR FROM GOVERNMENT PROPERTY.
9. REFERENCE MANUFACTURE'S TECHNICAL SPECIFICATION FOR INSTALLATION INSTRUCTIONS. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE INTERNATIONAL BUILDING CODE (IBC), NCDOT SPECIFICATIONS AND NORFOLK SOUTHERN SPECIFICATIONS.
10. CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH ROICC. NOTIFY THE ROICC IMMEDIATELY OF HIDDEN DAMAGE OR POTENTIAL REPAIR REQUIREMENTS THAT ARE NOT SPECIFIED WITHIN THIS SCOPE AND DISCOVERED DURING THE COURSE OF WORK.
11. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL (INCLUDING PEDESTRIAN TRAFFIC) NECESSARY TO SAFELY CONDUCT CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL CONDUCT OPERATIONS SO AS TO CAUSE THE LEAST POSSIBLE INTERFERENCE WITH NORMAL OPERATIONS TO THE MAXIMUM EXTENT POSSIBLE.
12. JOBSITE SHALL BE KEPT CLEAR OF ALL DEBRIS AND TRASH. PROPERLY REMOVE, HAUL AND DISPOSE OF ALL DEBRIS. THE CONTRACTOR SHALL NOT UTILIZE THE DUMPSTER PROVIDED BY CAMP LEJEUNE FOR THE OCCUPANTS OF THE BUILDING.
13. THE CAMP LEJEUNE LANDFILL MAY NOT BE UTILIZED FOR THE DISPOSAL OF BUILDING MATERIAL.
14. CONTRACTOR MUST CONTACT CAMP LEJEUNE FIRE AND EMERGENCY SERVICES PRIOR TO CONDUCTING ANY HOT WORKS ON THE SITE BY CALLING 910-451-3004/5. A TRAINED PERSON MUST BE SOLELY ASSIGNED AS FIRE WATCH DURING AND FOR A MINIMUM OF 60 MINUTES AFTER THE COMPLETION OF ANY HOT WORKS AND MUST HAVE A METHOD OF CONTACTING 911. TWO 10LBS ABC FIRE EXTINGUISHERS MUST BE AVAILABLE ON SITE DURING HOT WORK AND DURING THE 60 MINUTES FOLLOWING COMPLETION OF HOT WORK. COVER OR REMOVE ANY COMBUSTIBLE MATERIALS NEARBY AND FOLLOW NFPA STANDARDS FOR WORK.

HAZARDOUS MATERIALS

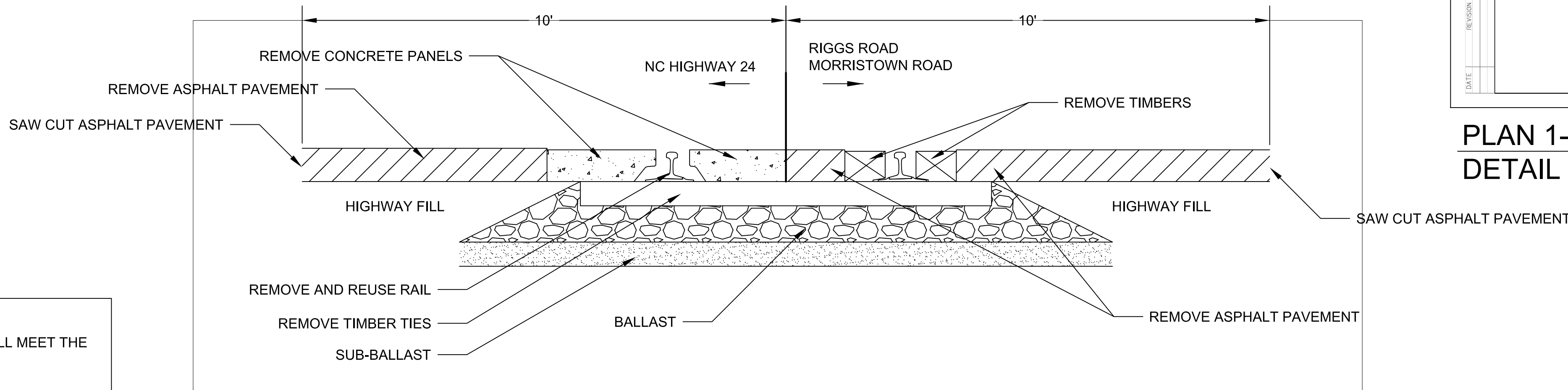
1. NO HAZARDOUS MATERIALS HAVE BEEN IDENTIFIED AT THE SITE.



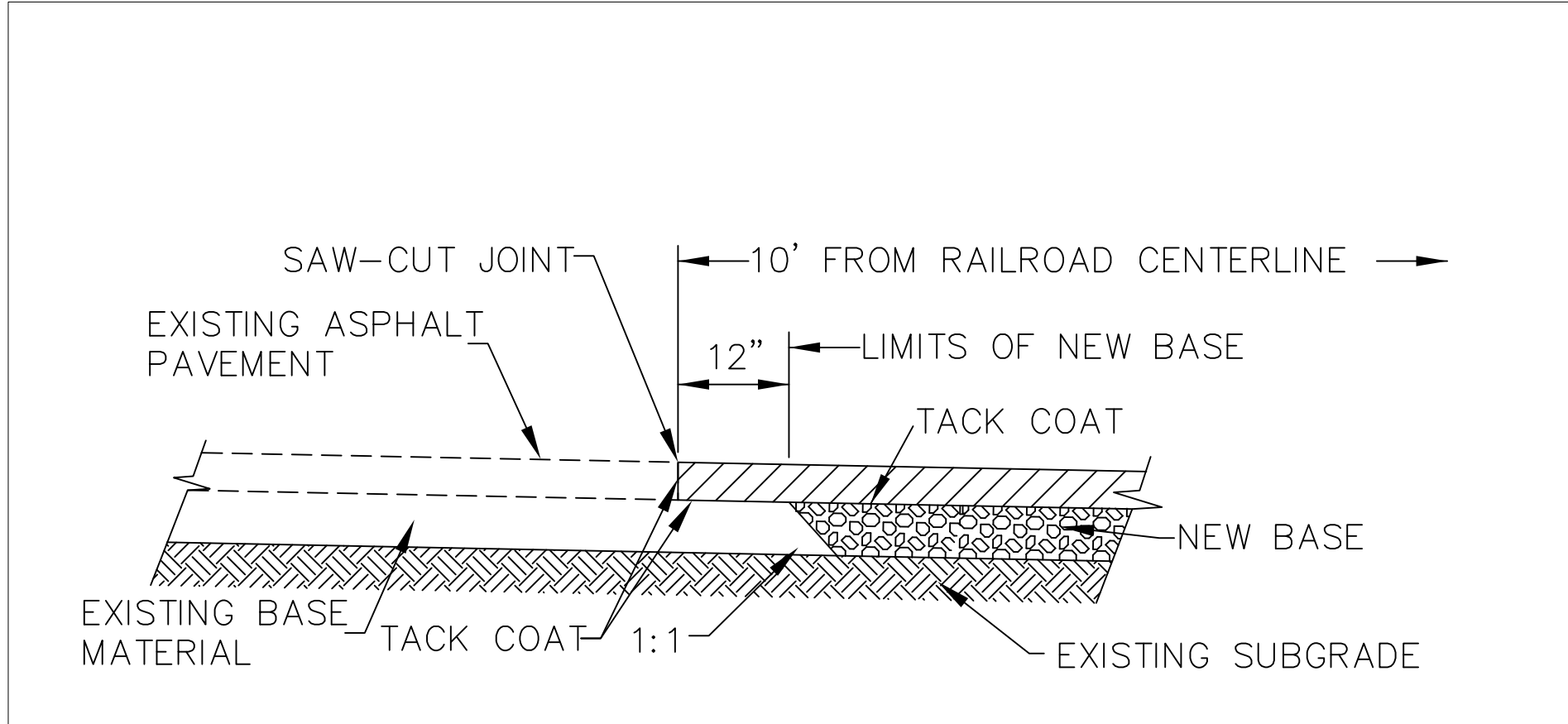
PLAN 7-5B - HIGHWAY GRADE CROSSING
DETAIL - NOT TO SCALE



C
C001
HIGHWAY ROADWAY
SECTION - NOT TO SCALE

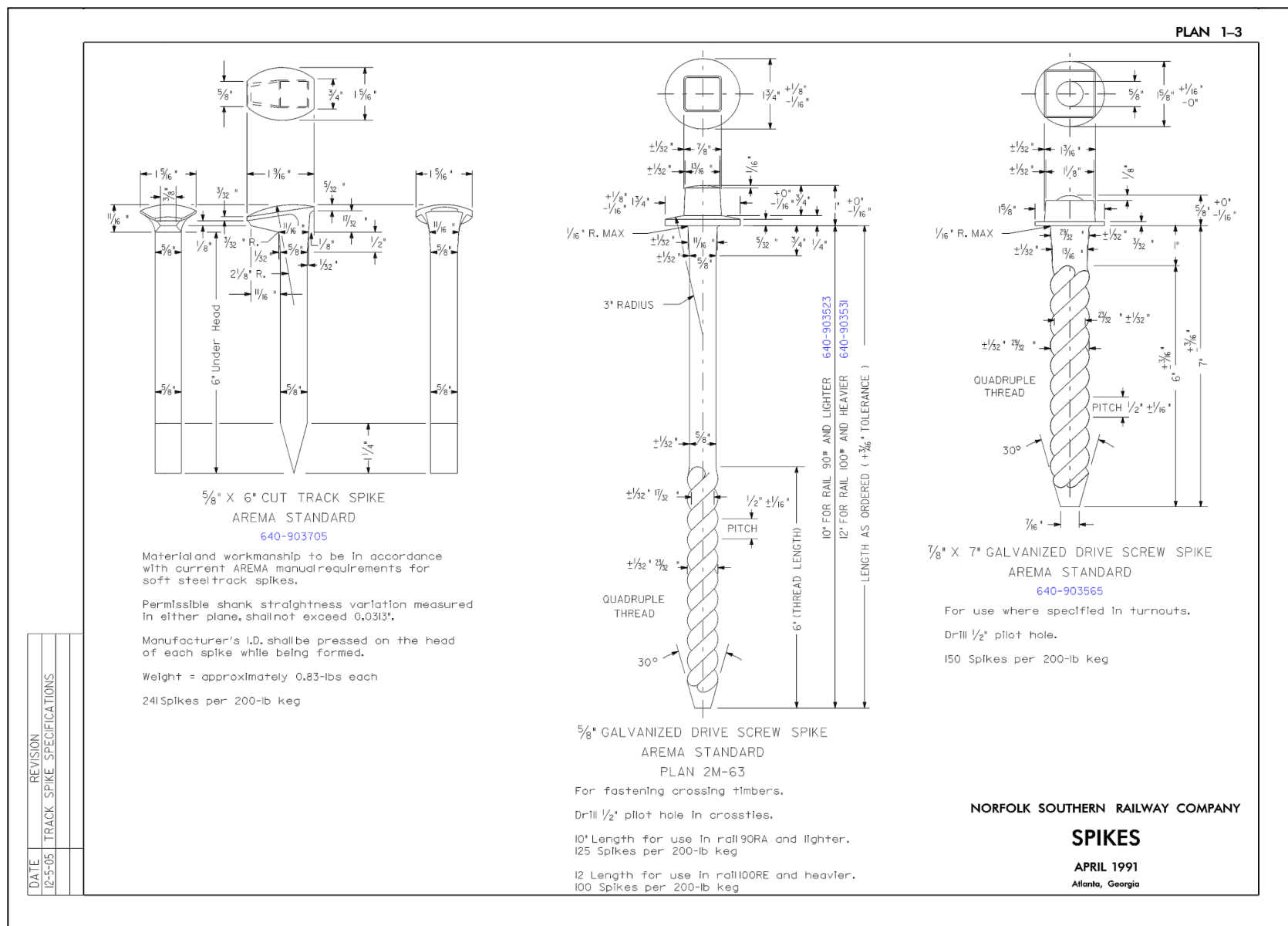


A
C001
DEMOLITION PLAN
SECTION - NOT TO SCALE

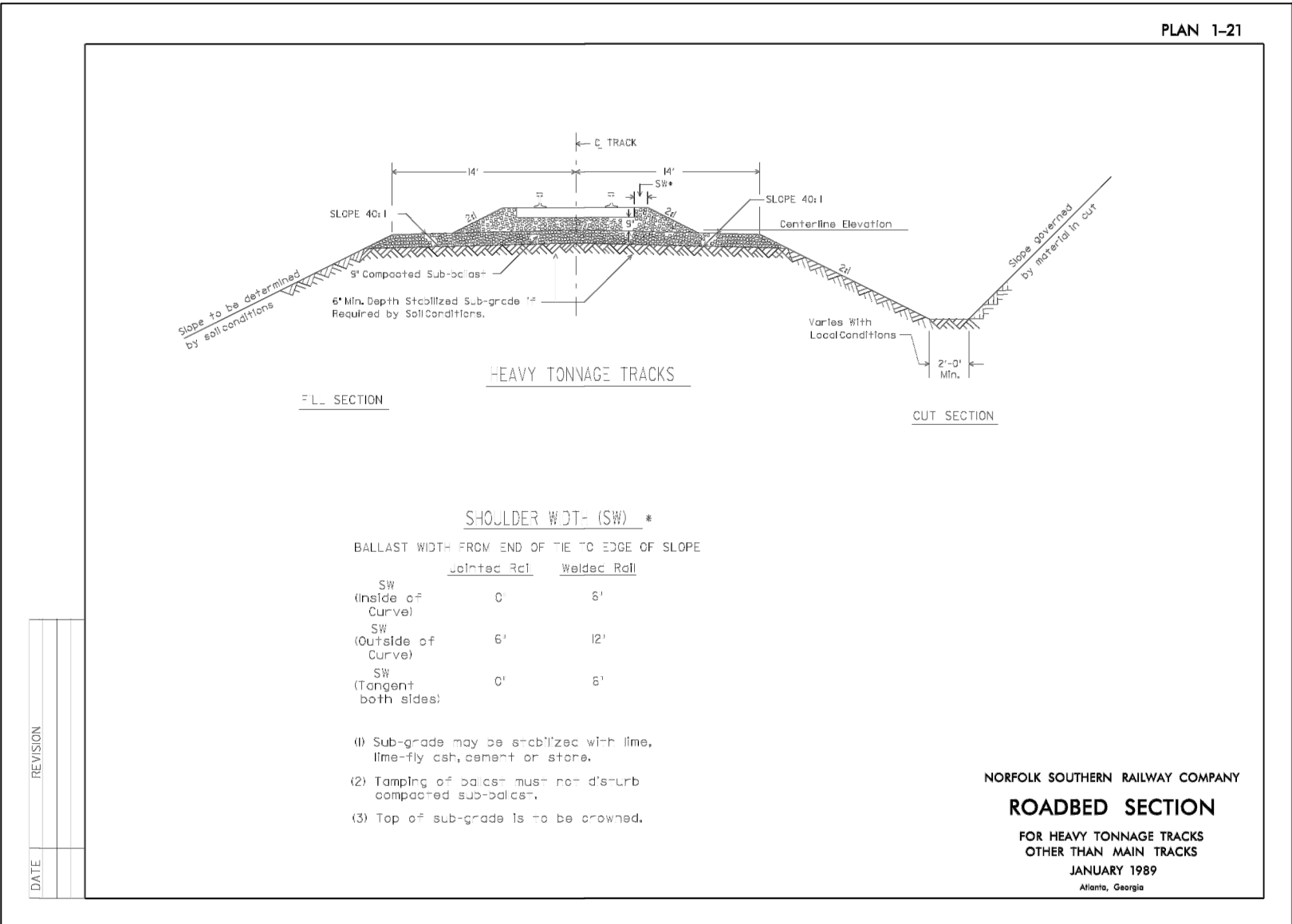


B
C001
SAW CUT DETAIL
SECTION - NOT TO SCALE

DATE	REVISION	DESCRIPTION



PLAN 1-3 SPIKES
DETAIL - NOT TO SCALE



PLAN 1-21 - ROADBED SECTION
DETAIL - NOT TO SCALE

SUB-BALLAST SPECIFICATIONS

1. SUB-BALLAST SHALL BE CRUSHER-RUN DENSE GRADED AGGREGATE, PREFERABLY LIMESTONE OR GRANITE MATERIAL AND SHALL MEET THE REQUIREMENTS AS SET OUT IN CHAPTER 1, PART 2, SECTION 2.11 "SUB-BALLAST SPECIFICATIONS" OF THE AREMA MANUAL.
2. GRADATION AS FOLLOWS:

SIEVE SIZE	2"	1"	3/8"	#10	#40	#200
% PASSING (OPTIMUM)	100	95	67	38	21	7
% PASSING (PERMISSIBLE RANGE)	100	90-100	50-84	26-50	12-30	0-10

3. THE SUB-BALLAST SHALL BE COMPACTED TO 95 PERCENT OF ITS MAXIMUM DRY DENSITY AND HAVE A MINIMUM DEPTH OF 9 INCHES.

BALLAST SPECIFICATIONS

1. MATERIAL FOR BALLAST SHALL BE CLEAN CRUSHED STONE WITH A MINIMUM DEPTH OF 9 INCHES.
2. STONE FOR USE AS BALLAST SHALL BE GRANITE CRUSHED STONE CONFORMING TO THE REQUIREMENTS OF NORFOLK SOUTHERN SPECIFICATION NO. 702 (2009) AND FURNISHED BY A NORFOLK SOUTHERN APPROVED QUARRY.
3. LIMESTONE BALLAST CONFIRMING TO NORFOLK SOUTHERN SPECIFICATION NO. 702 (2009) MAY BE USED SUBJECT TO APPROVAL OF NORFOLK SOUTHERN CHIEF ENGINEER.
4. IF MATERIAL MEETING THIS SPECIFICATION IS NOT AVAILABLE, LIMESTONE BALLAST MEETING AREMA SPECIFICATIONS MAY BE USED IF APPROVED BY NORFOLK SOUTHERN CHIEF ENGINEER.
5. BALLAST WILL BE #3 MODIFIED BALLAST AND SHALL CONFORM TO THE FOLLOWING GRADATION:

SIEVE SIZE	2-1/2"	2"	1-1/2"	1"	3/4"	1/2"	3/8"	#4	#200
#3 MODIFIED BALLAST (% PASSING)	100	95-100	30-65	0-15	-	0-5	-	-	0.5 MAX

TIMBER TIES SPECIFICATIONS

1. TIES SHALL BE NEW AND CONFORM TO NORFOLK SOUTHERN QUALITY ASSURANCE SPECIFICATION FOR TIES AND TIMERS, SECTION 2, NORFOLK SOUTHERN SPECIFICATIONS FOR WOODEN INDUSTRIAL GRADE CROSSTIES, REVISED MAY 1, 2017.
2. TIES SHALL BE PURCHASED FROM A NORFOLK SOUTHERN APPROVED SUPPLIER. CERTIFICATION OF THE TIES MUST BE FURNISHED TO NORFOLK SOUTHERN FROM THE CONTRACTOR AND/OR SUPPLIER BASED ON NORFOLK SOUTHERN SPECIFICATIONS.
3. TIES SHALL BE PLACED ON THE PREPARED SUB-BALLAST AT RIGHT ANGLES OR NORMAL TO THE CENTERLINE OF THE TRACK. TIES SHALL BE SPACED 20 INCHES, CENTER TO CENTER (60 TIES PER 100 FEET) WITH THE ENDS OF THE TIES ALONG ONE SIDE OF THE CENTERLINE ON TANGENT TRACK AND ALONG THE INSIDE ON CURVED TRACK BEING PLACED 1" 10 $\frac{3}{8}$ " FROM THE GAGE OF THE NEAREST RAIL.

DISCLOSURE OF INFORMATION

CONTRACTOR SHALL COMPLY AS FOLLOWS:

- (A) THE CONTRACTOR SHALL NOT RELEASE TO ANYONE OUTSIDE THE CONTRACTOR'S ORGANIZATION ANY UNCLASSIFIED INFORMATION, REGARDLESS OF MEDIUM (E.G., FILM, TAPE, DOCUMENT), PERTAINING TO ANY PART OF THIS CONTRACT OR ANY PROGRAM RELATED TO THIS CONTRACT, UNLESS:
 - (1) THE CONTRACTING OFFICER HAS GIVEN PRIOR WRITTEN APPROVAL; OR
 - (2) THE INFORMATION IS OTHERWISE IN THE PUBLIC DOMAIN BEFORE THE DATE OF RELEASE.

- (B) REQUESTS FOR APPROVAL SHALL IDENTIFY THE SPECIFIC INFORMATION TO BE RELEASED, THE MEDIUM TO BE USED, AND THE PURPOSE FOR THE RELEASE. THE CONTRACTOR SHALL SUBMIT ITS REQUEST TO THE CONTRACTING OFFICER AT LEAST 45 DAYS BEFORE THE PROPOSED DATE FOR RELEASE.

- (C) THE CONTRACTOR AGREES TO INCLUDE A SIMILAR REQUIREMENT IN EACH SUBCONTRACT UNDER THIS CONTRACT. SUBCONTRACTORS SHALL SUBMIT REQUESTS FOR AUTHORIZATION TO RELEASE THROUGH THE PRIME CONTRACTOR TO THE CONTRACTING OFFICER.

C001

DEPARTMENT OF THE NAVY NAVAL FACILITIES ENGINEERING COMMAND
MARINE CORPS BASE
CAMP LEJEUNE, NORTH CAROLINA

21-0027
REPAIR RAIL ROAD CROSSING
ON PUBLIC ROADS

SCOPE OF WORK

DES. DD ZIMBELMAN
DR. DD ZIMBELMAN
CHK. JP REED, PE
SUBMITTED BY: DD ZIMBELMAN
DESIGN DIR. T. BURTON, PE

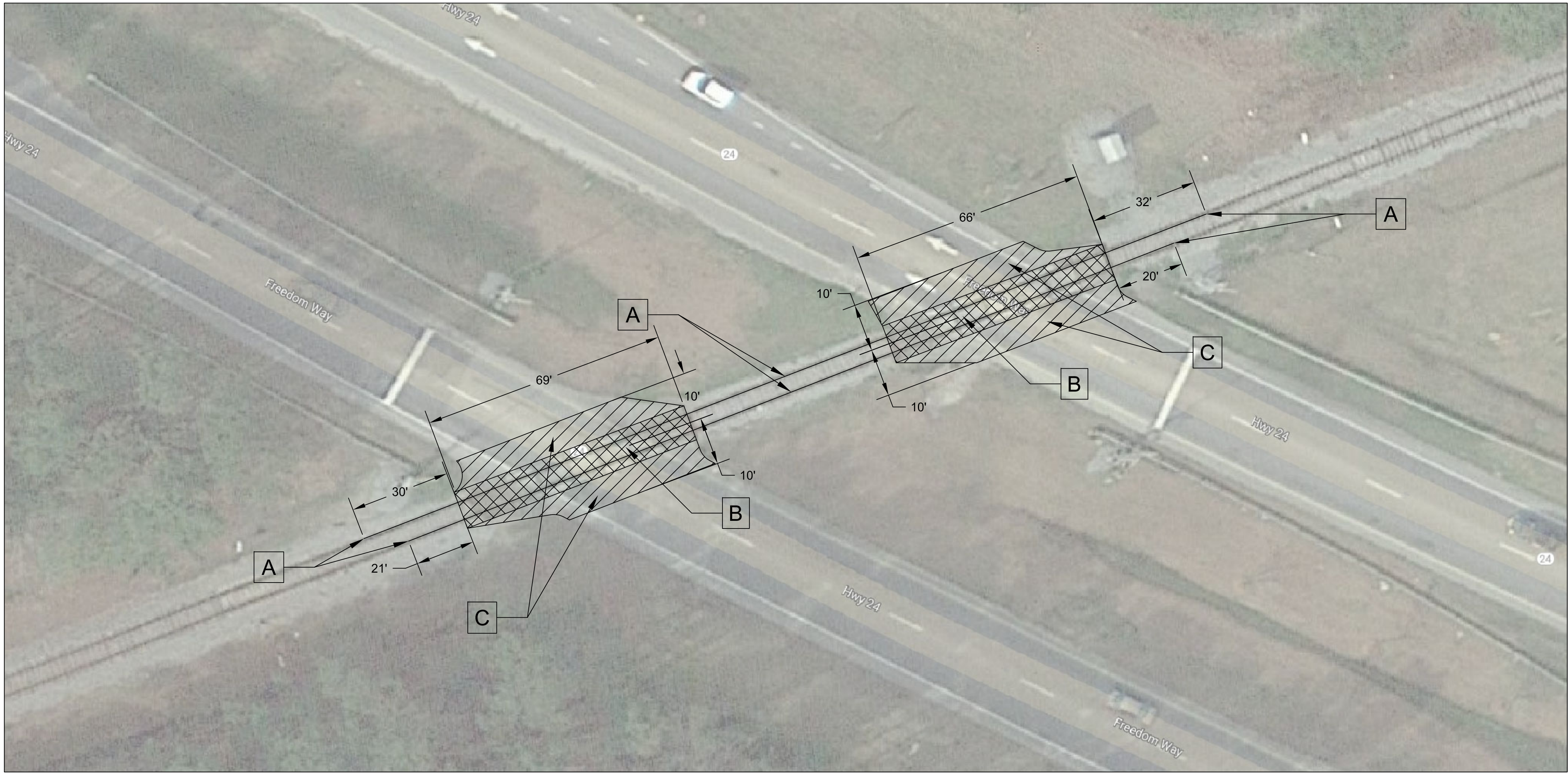
DATE: 08/20/2021

SIZE CODE IDENT NO. PWO DRAWING NO.
E180091 60035487

CONST. CONTR. NO. N40085-21-B-0027

SCALE: AS SHOWN SPEC. 05-21-0027 SHEET 2 OF 5

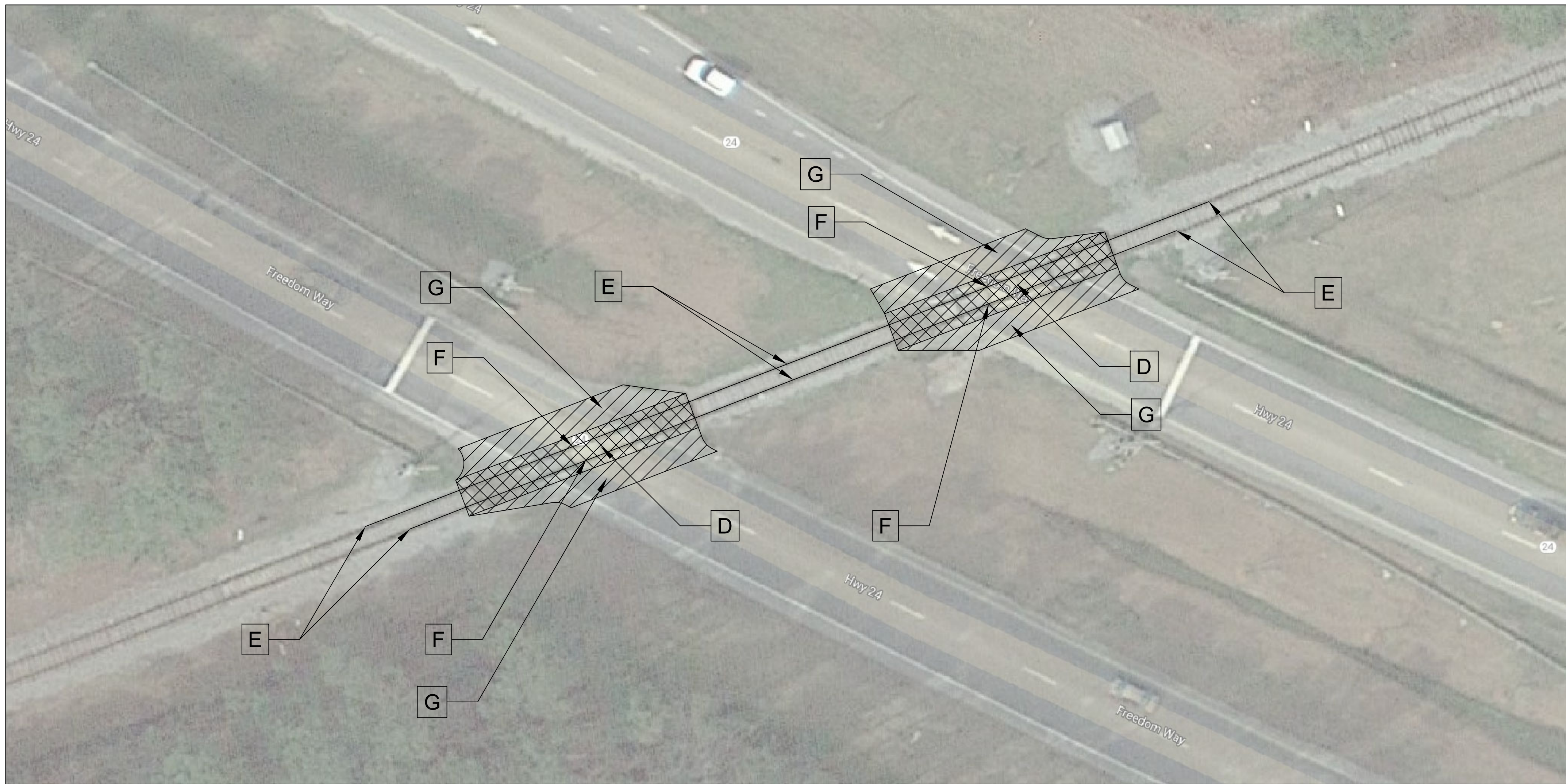
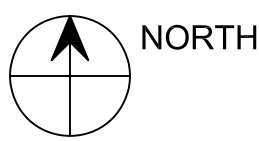
DATE	REVISION	DESCRIPTION



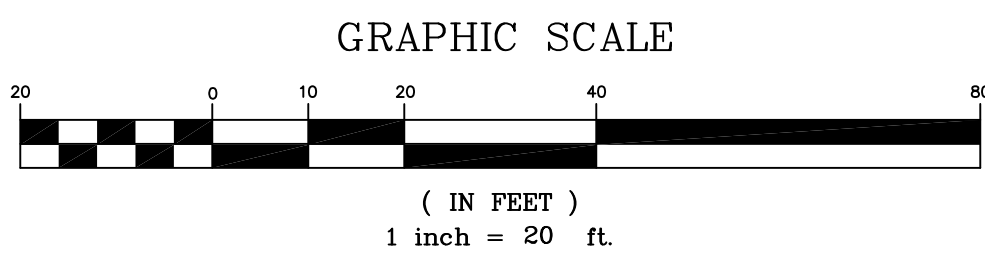
KEYED NOTES:

- A** REMOVE EXISTING RAIL FROM NEAREST JOINT OF TRACK SECTION. PROTECT. RAIL REMOVED WILL BE RE-USED.
- B** REMOVE CONCRETE RAIL ROAD CROSSING PANELS. REMOVE OLD TIMBER TIES AND BALLAST.
- C** SAWCUT PAVEMENT MIN OF 10' FROM CENTERLINE OF RAIL LINE AND REMOVE.
- D** ENSURE A 9" SUB-BALLAST UNDER FULL WIDTH AND LENGTH OF CROSSING. INSTALL NEW 9" BALLAST. INSTALL NEW TIES, TIE PLATES AND SPIKES.
- E** REINSTALL RAIL PREVIOUSLY REMOVED.
- F** INSTALL NEW RUBBER FLANGeway PER DETAIL PLAN 7-5B
- G** REPAVE ROADWAY AS PER DETAIL C/C001

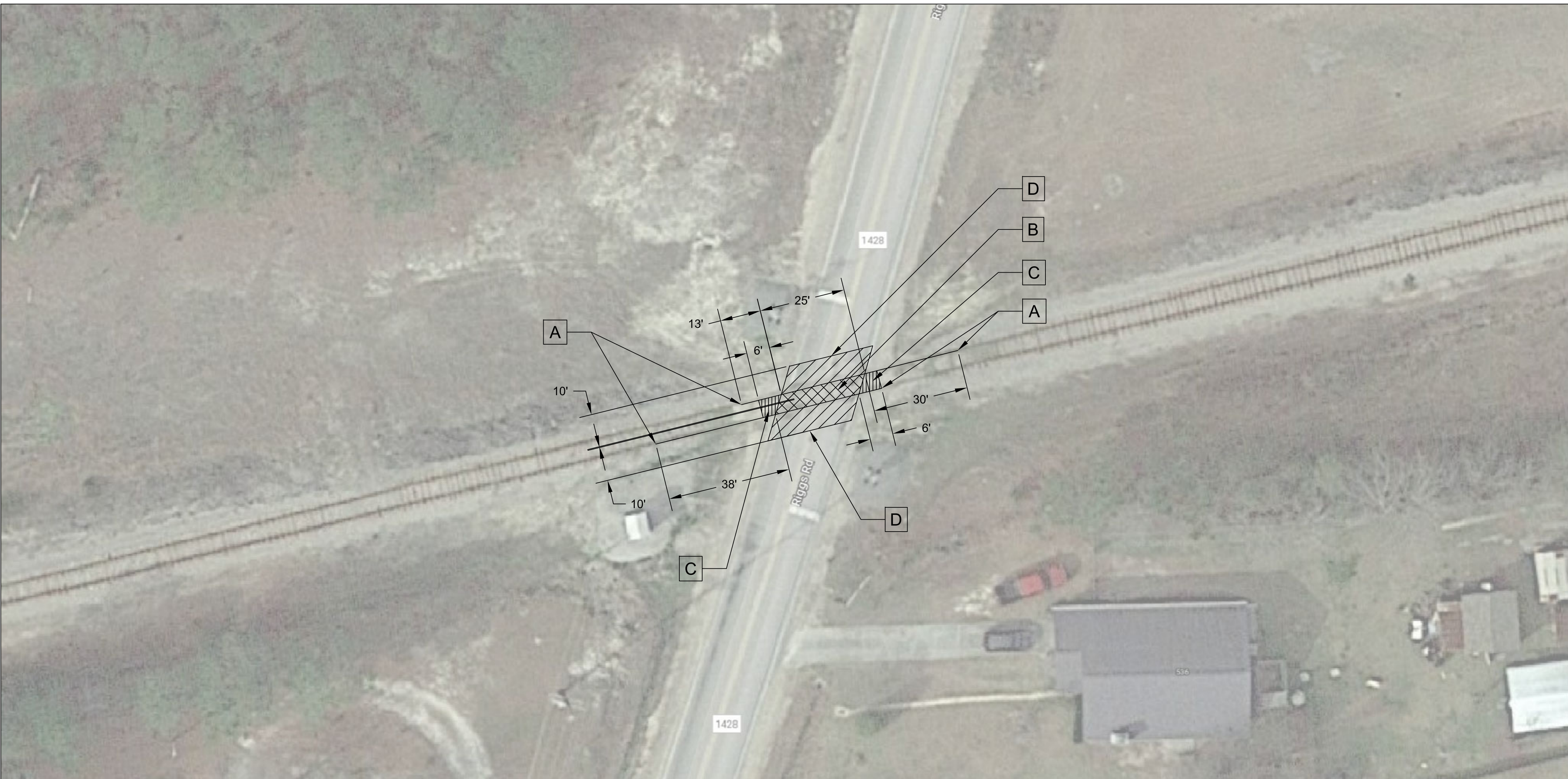
D1 NC HIGHWAY 24 - DEMOLITION
C101 PLAN - 1" = 20'



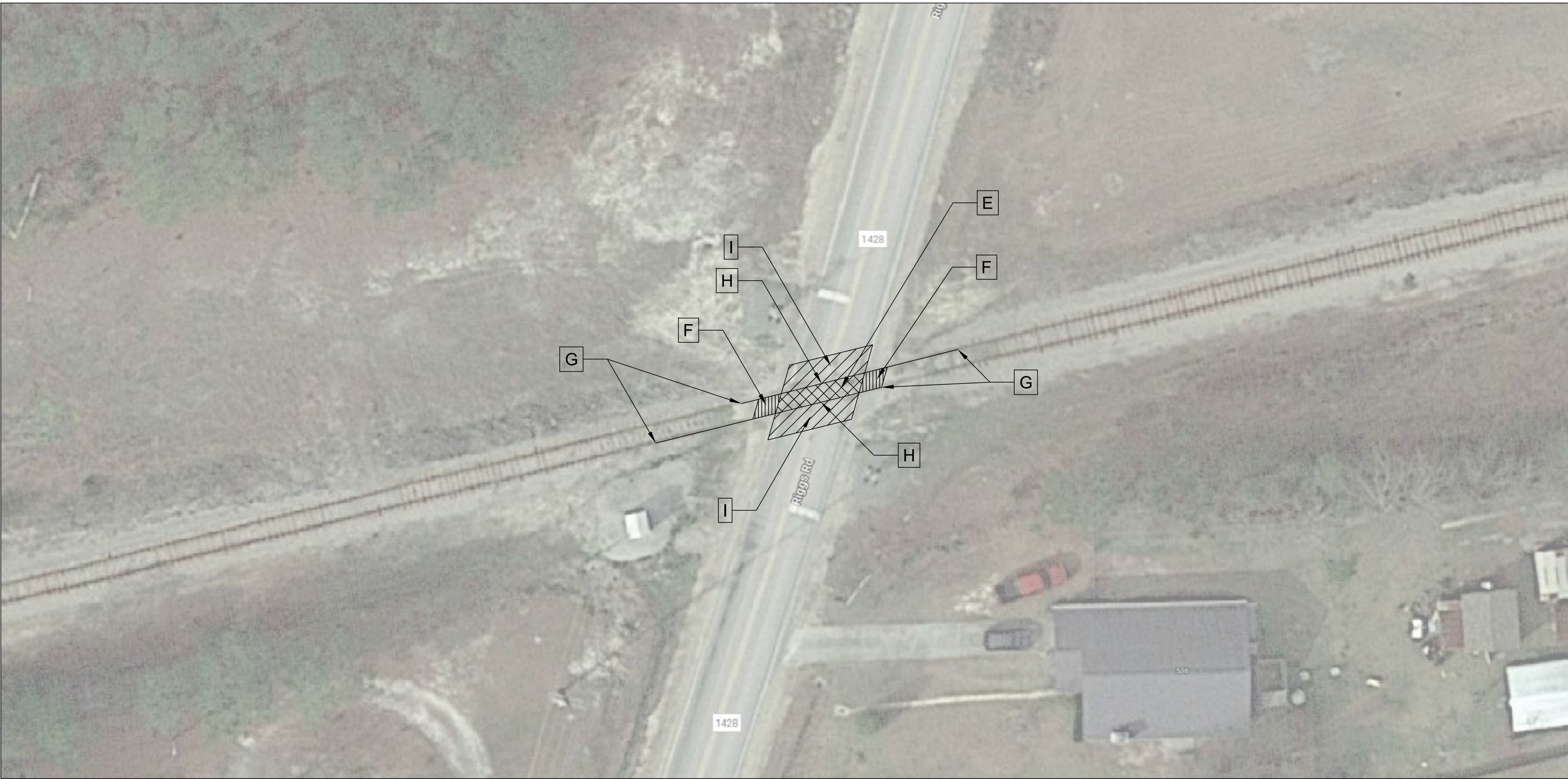
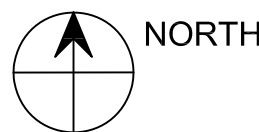
D2 NC HIGHWAY 24 - NEW WORK
C101 PLAN - 1" = 20"



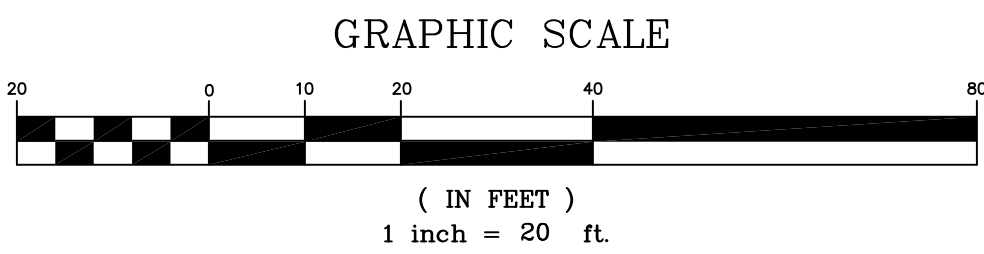
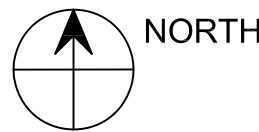
DISCLOSURE OF INFORMATION CONTRACTOR SHALL COMPLY AS FOLLOWS: (A) THE CONTRACTOR SHALL NOT RELEASE TO ANYONE OUTSIDE THE CONTRACTOR'S ORGANIZATION ANY UNCLASSIFIED INFORMATION, REGARDLESS OF MEDIUM (E.G., FILM, TAPE, DOCUMENT), PERTAINING TO ANY PART OF THIS CONTRACT OR ANY PROGRAM RELATED TO THIS CONTRACT, UNLESS: (1) THE CONTRACTING OFFICER HAS GIVEN PRIOR WRITTEN APPROVAL; OR (2) THE INFORMATION IS OTHERWISE IN THE PUBLIC DOMAIN BEFORE THE DATE OF RELEASE. (B) REQUESTS FOR APPROVAL SHALL IDENTIFY THE SPECIFIC INFORMATION TO BE RELEASED, THE MEDIUM TO BE USED, AND THE PURPOSE FOR THE RELEASE. THE CONTRACTOR SHALL SUBMIT ITS REQUEST TO THE CONTRACTING OFFICER AT LEAST 45 DAYS BEFORE THE PROPOSED DATE FOR RELEASE. (C) THE CONTRACTOR AGREES TO INCLUDE A SIMILAR REQUIREMENT IN EACH SUBCONTRACT UNDER THIS CONTRACT. SUBCONTRACTORS SHALL SUBMIT REQUESTS FOR AUTHORIZATION TO RELEASE THROUGH THE PRIME CONTRACTOR TO THE CONTRACTING OFFICER.		C101	
		DEPARTMENT OF THE NAVY NAVAL FACILITIES ENGINEERING COMMAND MARINE CORPS BASE CAMP LEJEUNE, NORTH CAROLINA	
DES. DD ZIMBELMAN DR. DD ZIMBELMAN CHK. JP REED, PE SUBMITTED BY: DD ZIMBELMAN DESIGN DIR. T. BURTON, PE		21-0027 REPAIR RAIL ROAD CROSSING ON PUBLIC ROADS NC HIGHWAY 24	
DATE: 8/23/2021		SIZE CODE IDENT NO. E180091	PWO DRAWING NO. 60035488
		SCALE: AS SHOWN SPEC. 05-21-0027	SHEET 3 OF 5



E1 RIGGS ROAD - DEMOLITION
C102 PLAN - 1" = 20'



E2 RIGGS ROAD - NEW WORK
C102 PLAN - 1"=20'



DATE	REVISION	DESCRIPTION

KEYED NOTES:

- [A] REMOVE EXISTING RAIL FROM NEAREST JOINT OF TRACK SECTION. PROTECT. RAIL REMOVED WILL BE RE-USED.
- [B] REMOVE ASPHALT AND OLD CROSSING TIMBERS . REMOVE OLD TIMBER TIES AND BALLAST.
- [C] REMOVE OLD TIMBER TIES AND BALLAST (BEYOND ROADWAY).
- [D] SAWCUT PAVEMENT MIN OF 10' FROM CENTERLINE OF RAIL LINE AND REMOVE.
- [E] ENSURE A 9" SUB-BALLAST UNDER FULL WIDTH AND LENGTH OF CROSSING. INSTALL NEW 9" BALLAST. INSTALL NEW TIES, TIE PLATES AND SPIKES.
- [F] ENSURE A 9" SUB-BALLAST BEYOND THE CROSSING. INSTALL NEW 9" BALLAST. INSTALL NEW TIES, TIE PLATE AND SPIKES.
- [G] REINSTALL RAIL PREVIOUSLY REMOVED.
- [H] INSTALL NEW RUBBER FLANGEWAY PER DETAIL PLAN 7-5B
- [I] REPAVE ROADWAY AS PER DETAIL C/C001

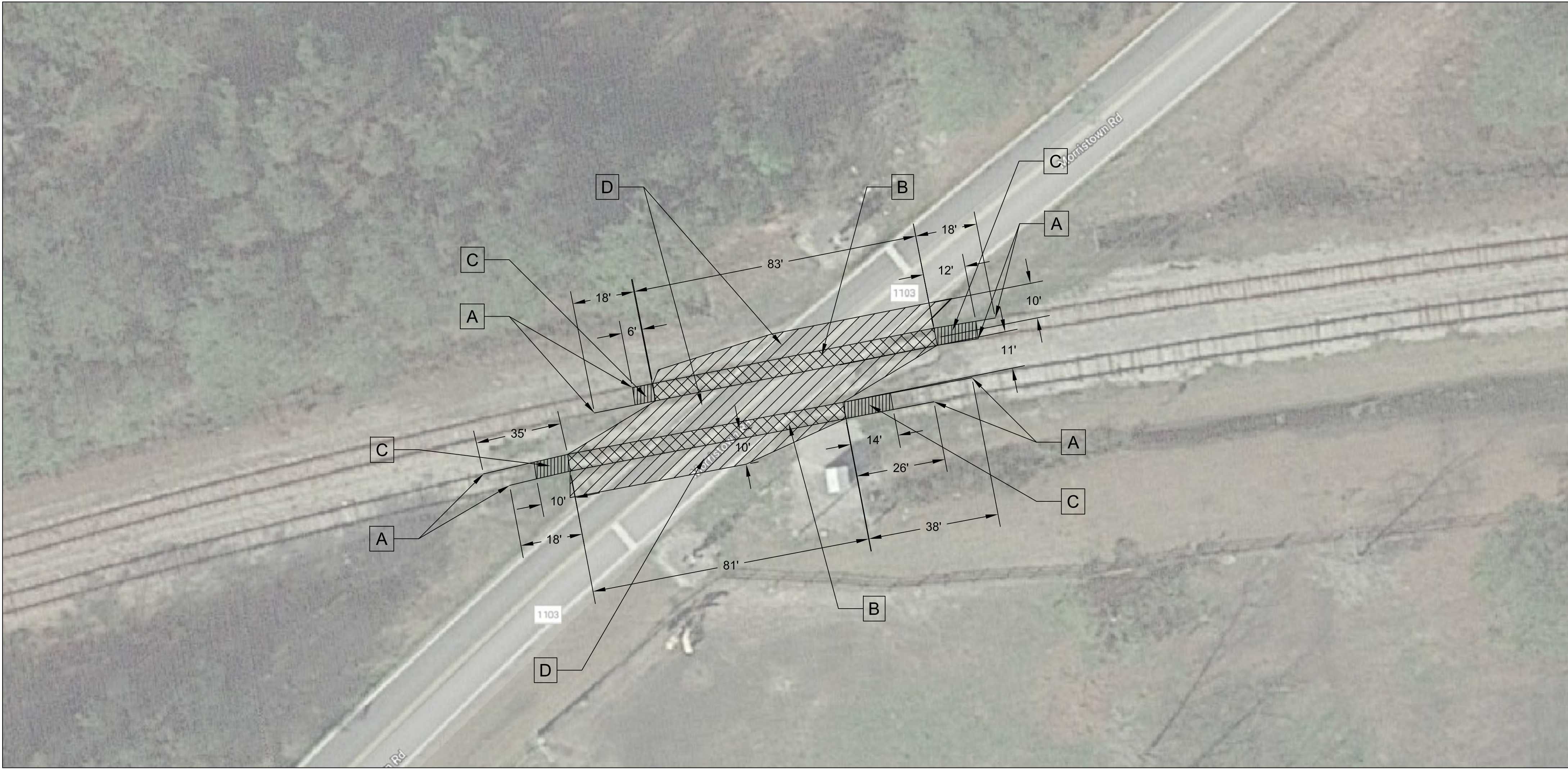
DISCLOSURE OF INFORMATION

CONTRACTOR SHALL COMPLY AS FOLLOWS:

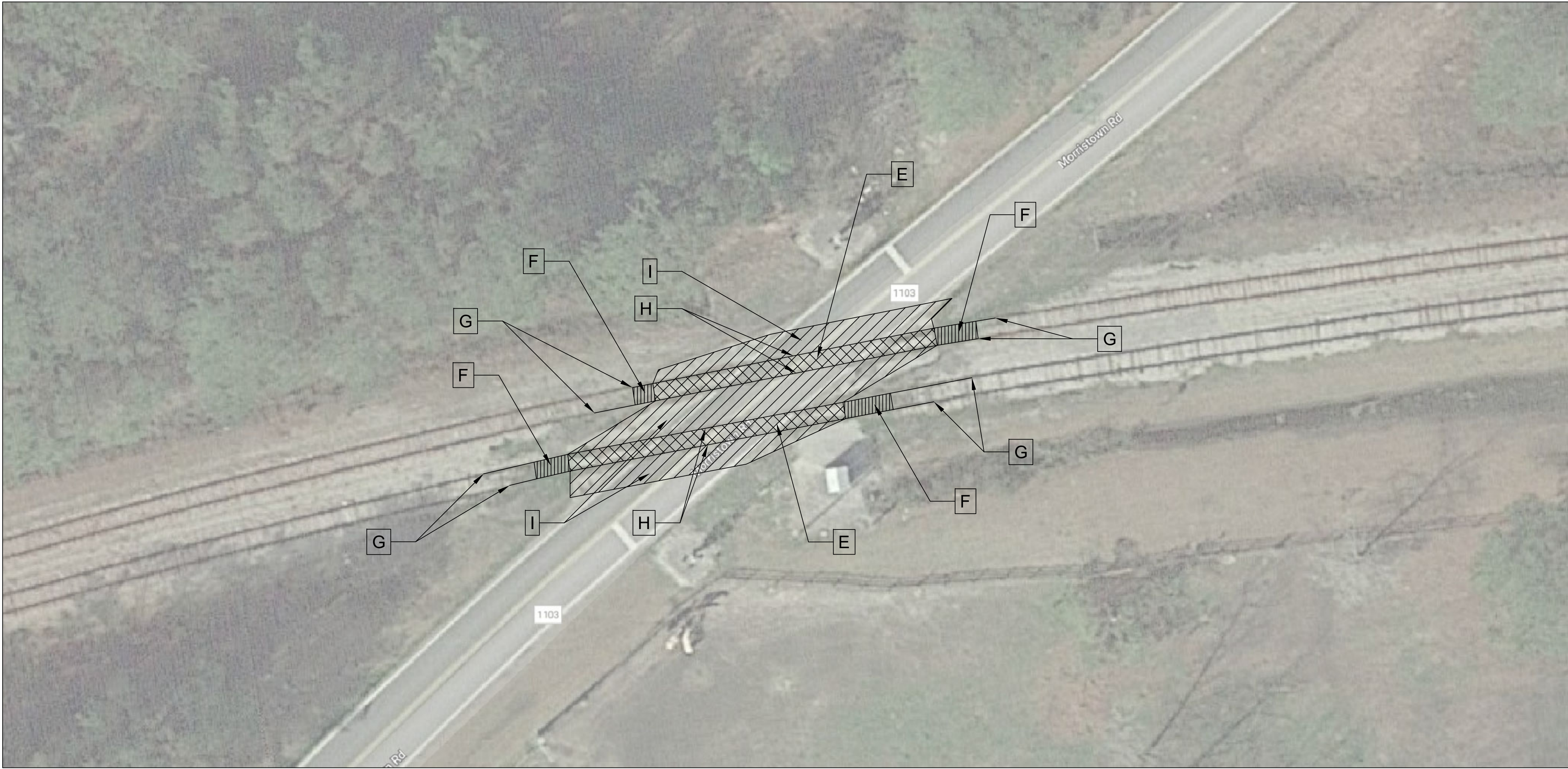
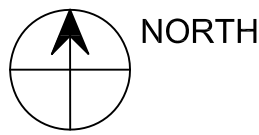
- (A) THE CONTRACTOR SHALL NOT RELEASE TO ANYONE OUTSIDE THE CONTRACTOR'S ORGANIZATION ANY UNCLASSIFIED INFORMATION, REGARDLESS OF MEDIUM (E.G., FILM, TAPE, DOCUMENT), PERTAINING TO ANY PART OF THIS CONTRACT OR ANY PROGRAM RELATED TO THIS CONTRACT, UNLESS:
- (1) THE CONTRACTING OFFICER HAS GIVEN PRIOR WRITTEN APPROVAL; OR
- (2) THE INFORMATION IS OTHERWISE IN THE PUBLIC DOMAIN BEFORE THE DATE OF RELEASE.
- (B) REQUESTS FOR APPROVAL SHALL IDENTIFY THE SPECIFIC INFORMATION TO BE RELEASED, THE MEDIUM TO BE USED, AND THE PURPOSE FOR THE RELEASE. THE CONTRACTOR SHALL SUBMIT ITS REQUEST TO THE CONTRACTING OFFICER AT LEAST 45 DAYS BEFORE THE PROPOSED DATE FOR RELEASE.
- (C) THE CONTRACTOR AGREES TO INCLUDE A SIMILAR REQUIREMENT IN EACH SUBCONTRACT UNDER THIS CONTRACT. SUBCONTRACTORS SHALL SUBMIT REQUESTS FOR AUTHORIZATION TO RELEASE THROUGH THE PRIME CONTRACTOR TO THE CONTRACTING OFFICER.

C102

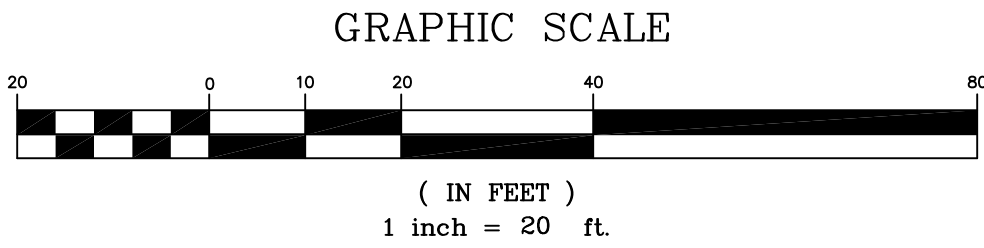
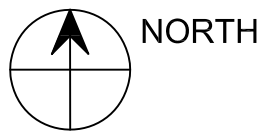
DES. DD ZIMBELMAN		DEPARTMENT OF THE NAVY NAVAL FACILITIES ENGINEERING COMMAND	
DR. DD ZIMBELMAN		MARINE CORPS BASE	
CHK. JP REED, PE		CAMP LEJEUNE, NORTH CAROLINA	
SUBMITTED BY: DD ZIMBELMAN		21-0027	
DESIGN DIR. T. BURTON, PE		REPAIR RAIL ROAD CROSSING ON PUBLIC ROADS	
DATE: 8/24/2021		RIGGS ROAD	
SIZE CODE IDENT NO. E180091		PWO DRAWING NO. 60035489	
CONST. CONTR. NO. N40085-21-B-0027		SCALE: AS SHOWN SPEC. 05-21-0027 SHEET 4 OF 5	



F1 MORRISTOWN ROAD - DEMOLITION
C103 PLAN - 1" = 20'



F2 MORRISTOWN ROAD - NEW WORK
C103 PLAN - 1" = 20'



DATE	REVISION	DESCRIPTION

KEYED NOTES:

- A REMOVE EXISTING RAIL FROM NEAREST JOINT OF TRACK SECTION. PROTECT. RAIL REMOVED WILL BE RE-USED.
- B REMOVE ASPHALT AND OLD CROSSING TIMBERS . REMOVE OLD TIMBER TIES AND BALLAST.
- C REMOVE OLD TIMBER TIES AND BALLAST (BEYOND ROADWAY).
- D SAWCUT PAVEMENT MIN OF 10' FROM CENTERLINE OF RAIL LINE AND REMOVE.
- E ENSURE A 9" SUB-BALLAST UNDER FULL WIDTH AND LENGTH OF CROSSING. INSTALL NEW 9" BALLAST. INSTALL NEW TIES, TIE PLATES AND SPIKES.
- F ENSURE A 9" SUB-BALLAST BEYOND THE CROSSING. INSTALL NEW 9" BALLAST. INSTALL NEW TIES, TIE PLATE AND SPIKES.
- G REINSTALL RAIL PREVIOUSLY REMOVED.
- H INSTALL NEW RUBBER FLANGEWAY PER DETAIL PLAN 7-58
- I REPAVE ROADWAY AS PER DETAIL C/C001

DISCLOSURE OF INFORMATION

CONTRACTOR SHALL COMPLY AS FOLLOWS:

- (A) THE CONTRACTOR SHALL NOT RELEASE TO ANYONE OUTSIDE THE CONTRACTOR'S ORGANIZATION ANY UNCLASSIFIED INFORMATION, REGARDLESS OF MEDIUM (E.G., FILM, TAPE, DOCUMENT), PERTAINING TO ANY PART OF THIS CONTRACT OR ANY PROGRAM RELATED TO THIS CONTRACT, UNLESS:
 - (1) THE CONTRACTING OFFICER HAS GIVEN PRIOR WRITTEN APPROVAL; OR
 - (2) THE INFORMATION IS OTHERWISE IN THE PUBLIC DOMAIN BEFORE THE DATE OF RELEASE.
- (B) REQUESTS FOR APPROVAL SHALL IDENTIFY THE SPECIFIC INFORMATION TO BE RELEASED, THE MEDIUM TO BE USED, AND THE PURPOSE FOR THE RELEASE. THE CONTRACTOR SHALL SUBMIT ITS REQUEST TO THE CONTRACTING OFFICER AT LEAST 45 DAYS BEFORE THE PROPOSED DATE FOR RELEASE.
- (C) THE CONTRACTOR AGREES TO INCLUDE A SIMILAR REQUIREMENT IN EACH SUBCONTRACT UNDER THIS CONTRACT. SUBCONTRACTORS SHALL SUBMIT REQUESTS FOR AUTHORIZATION TO RELEASE THROUGH THE PRIME CONTRACTOR TO THE CONTRACTING OFFICER.

C103

DES. DD ZIMBELMAN		DEPARTMENT OF THE NAVY NAVAL FACILITIES ENGINEERING COMMAND	
DR. DD ZIMBELMAN		MARINE CORPS BASE	
CHK. JP REED, PE		CAMP LEJEUNE, NORTH CAROLINA	
SUBMITTED BY: DD ZIMBELMAN		21-0027	
DESIGN DIR. T. BURTON, PE		REPAIR RAIL ROAD CROSSING ON PUBLIC ROADS	
DATE: 8/26/2021		MORRISTOWN ROAD	
SIZE CODE IDENT NO. E180091		PWO DRAWING NO. 60035490	
SCALE: AS SHOWN		CONST. CONTR. NO. N40085-21-B-0027	
SPEC. 05-21-0027		SHEET 5 OF 5	